



North Side Transfer Zone

Public Open House

June 10, 2015

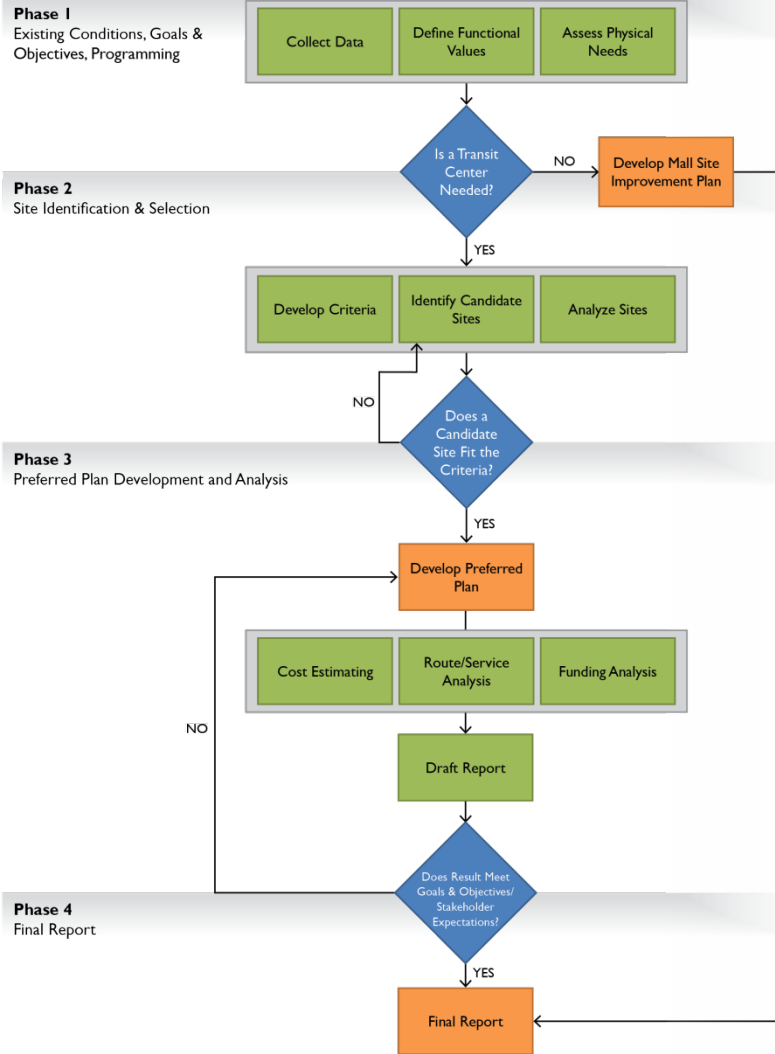


EXPERIENCE | Transportation

To determine the feasibility of a transit transfer zone in the vicinity of the Northwoods Mall commercial area

- ▀ Safety for Transfers
- ▀ Passenger Amenities
- ▀ CityLink Efficiency
- ▀ Ability to Grow for Future
- ▀ Identified in Operations & Transportation Plans

Project Scope and Schedule



Four Step Process

- Determine Requirements
- Identify Locations and Select Preferred Site
- Develop Site and Operations Changes
- Final Report

Two Public Meetings Held

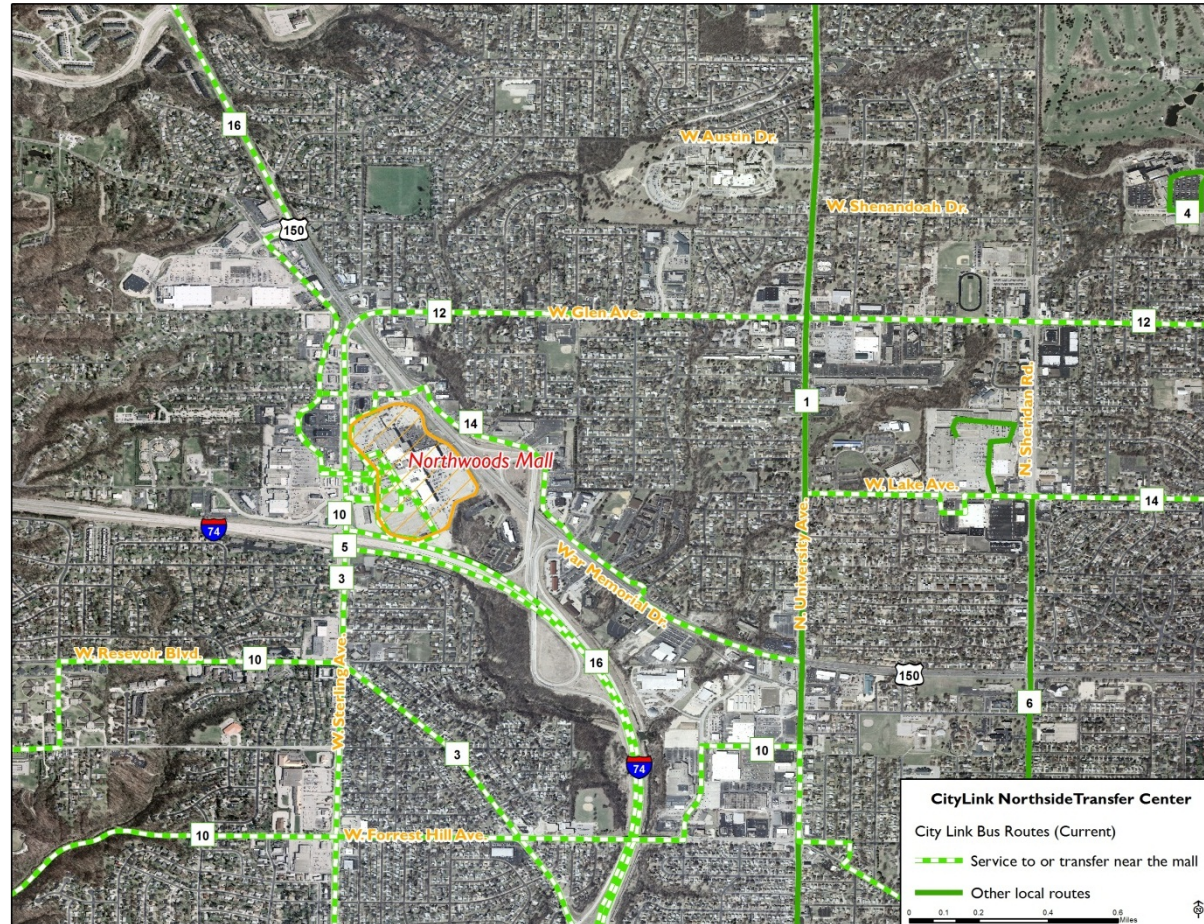
- April 16, 2014
- July 29, 2014

Project is at Phase 4

Project Background

■ Northwoods Mall area is a focal point for CityLink service

- 6 routes operate to the Mall area
- Need to have a second transfer point for efficient bus operations
- Want to improve passenger safety and provide passenger amenities



Primary Project Goals

- Provide a location to transfer the routes that serve the north side and converge in the Northwoods Mall area
- Provide ADA accessible and safe boarding areas for transfer between buses that minimizes travel between boarding points
- Provide for secure and sheltered passenger waiting with information and emergency communications
- Accommodate operator and supervisor facilities that minimizes time away from the bus
- Minimize operational cost impact to CityLink

Transfer Zone Components - Minimum Requirements

■ Site Components

- Bus Bays
 - 8 sawtooth bays
- Boarding Platforms
 - Covered
 - Benches, trash cans, information kiosk
 - Bike racks
- Landscaping/fencing



Janesville Transit Center

■ Building Components

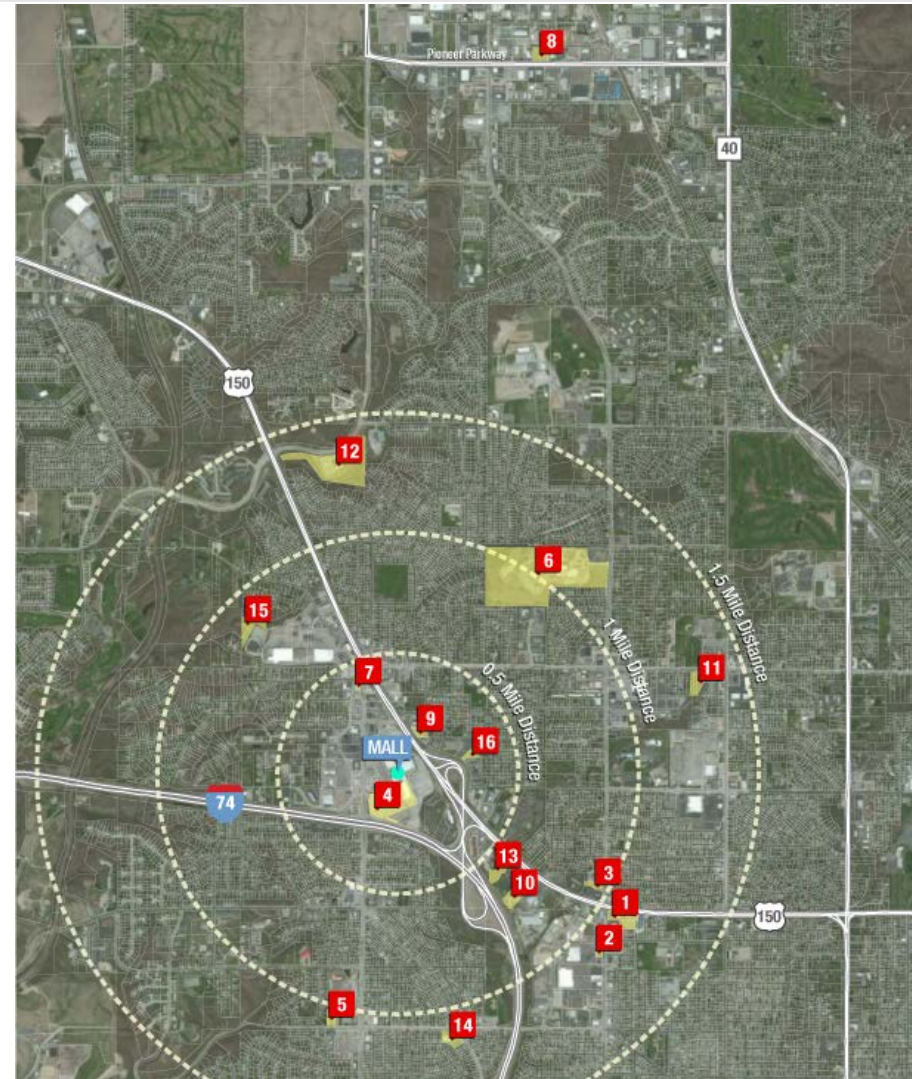
- Patron waiting area
- Operator restrooms
- Electrical/mechanical room



Evaluated Sites

16 Sites Reviewed:

- Site 1: Corner of University /War Memorial
- Site 2: University Plaza Our Parcel
- Site 3: Goodwill Parking Lot
- Site 4: Old Firestone building
- Site 5: State Farm Building
- Site 6: ICC Campus
- Site 7: Former Pizza Hut
- Site 8: Pioneer Park/University Area
- Site 9: Brandywine Drive Retail
- Site 10: 1620 War Memorial
- Site 11: 812 Glen Avenue
- Site 12: Allen Road
- Site 13: 1720 War Memorial
- Site 14: Former Bowling Alley
- Site 15: Back Lot of Shopping Center
- Site 16: CEFCU Site (added after Public Meeting No. 2)



Site Selection Criteria

► Sites Ranked by:

- Size/Shape of Parcel (1 acre minimum for full program)
- Access Conditions
- Environmental Concerns
- Transit System Connectivity
- Land Use Context
- Available Destinations
- Multimodal Connectivity
- Anticipated Land Cost
- Anticipated Development Cost
- Availability

Three Preferred Sites After Public Meeting No. 2

➤ Site 5- State Farm Building

- Not preferred due to high costs of demolition of existing building and proximity to residential area

➤ Site 7- Former Pizza Hut Site

- Under consideration but not ranked the highest due to size of site

➤ Site 16 – CEFCU Site

- Recommended site
- Located across from Northwoods Mall on an access road (4310 N. Brandywine)
- Pedestrian access not ideal but site can accommodate building footprint



North Side Transfer Zone

PREFERRED SITES



SITE 5: State Farm Building

An available parcel that was formerly the State Farm office building and insurance inspection center. It has a large building on site.



SITE 7: Former Pizza Hut

An outlot that was formerly a Pizza Hut near the intersection of Sterling and War Memorial. Reduced program (6 bays) due to site constraints.

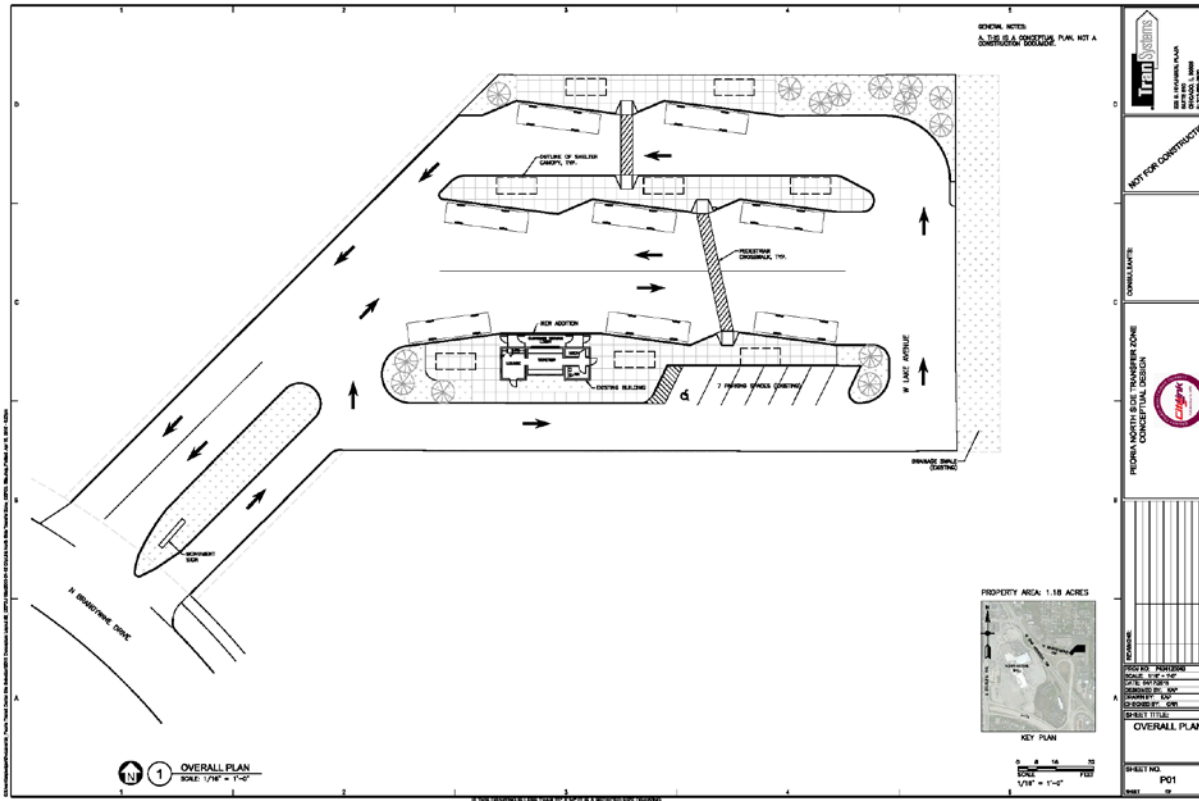
Selected Site – CEFCU Site



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Selected Site – CEFCU Site



Site Characteristics

- 1.2 acre site
- Renovated/expanded former bank building to be used as:
 - Passenger waiting area
 - Customer service area
 - Driver facilities
- 8 sawtooth bays for 40' buses
- 7 space parking lot
- 10-inch high boarding platforms
- Canopy over waiting area
- Security cameras
- Information system

Conceptual Cost Estimate

\$2,054,130

(Not including land acquisition, environmental remediation)

CityLink Operational Adjustments

- Introduction of a new Transfer Zone would no longer require bus layovers at Northwoods Mall
- Buses would not stop along perimeter roads, such as Sterling Avenue, for riders to transfer between routes
- Transfer Zone allows for easier and safer transfers and improved connectivity improving on time performance and operational expenditures
- Routes using Transfer Zone will be:
 - #3-Parkview
 - #5-Main
 - #10-Sterling
 - #12-Heights
 - #14-Wisconsin
 - #16-Northwest Express

Next Steps

➤ Secure Funding

- Federal Sources (20%+local match):
 - Section 5307 Urbanized Area Formula Program
 - Section 5310 Enhanced Mobility of Seniors and Individuals with Disabilities
- State Sources (20% local match):
 - Illinois Transportation Enhancement Program (ITEP)- only for pedestrian/bike connections
 - Illinois Jobs Now! Downstate Transportation Improvement Fund (DTIF)
- Local Funding
 - Transit Development District (variation of a TIF)
 - Real Estate Transfer Tax
 - Parking Tax
- Public –Private Partnership
 - Design-Build

Next Steps (continued)

- Purchase Property
- Environmental Reconnaissance
- Design Transfer Zone
- Construct Transfer Zone



▀ Questions?

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